



**Eaton County  
Road  
Commission**

May 1, 2017

Duane A. Eldred  
**Chair**

**NOTICE TO BIDDERS**

Timothy J. Lamoreaux  
**Vice Chair**

Jerry L. Frazier  
**Member**

Sealed proposals will be received by the Board of County Road Commissioners of the County of Eaton, Michigan at 1112 Reynolds Road, Charlotte, MI 48813, until 9:00 A.M., Tuesday, June 13, 2017 at which time and place proposals will be publicly opened and read aloud for furnishing and delivering the following:

Benjamin S. Lyons  
**Member**

**HMA RESURFACING**

Dorothy E. Maxwell  
**Member**

**Contract D – Approximately 18,100 Tons HMA 36A; 1,030 Tons HMA 36A Wedging; 9,170 Tons HMA 36A Leveling. Approximately 2,000 SYD cold milling, pavement removal, aggregate base, granular subbase, drainage structure cover adjustments, monument box adjustments, temporary pavement marking and gravel shoulders at various locations in Eaton County, Michigan.**

Blair E. Ballou, P.E.  
**Engineer-Manager**

The Eaton County Road Commission will consider award of the contracts after review by staff.

Lori N. Friedlis, CPA  
**Director of Finance**

Further information may be obtained at the offices of the undersigned, 1112 Reynolds Road, Charlotte, MI, 48813, Telephone (517) 543-1630 or (877) 883-2866. Plans, specifications, and bid blanks are available at our office.

1112 Reynolds Road  
Charlotte, MI 48813

All proposals must be submitted in sealed envelopes bearing the name of the bidder and clearly marked with a notation indicating the project on which the bid is submitted. **All bids must be submitted on the enclosed Bid Blank.**

Telephone  
517-543-1630  
Toll-Free  
877-883-2866

The Board reserves the right to accept or reject any or all bids, to waive irregularities in any proposal, and to make the award in any manner deemed in the best interest of the County of Eaton.

Fax  
517-543-2608

**BOARD OF COUNTY ROAD COMMISSIONERS  
OF THE COUNTY OF EATON, MICHIGAN**

Email  
ecrc@eatoncountyroadd.com

Duane A. Eldred, Chair  
Timothy J. Lamoreaux, Vice Chair  
Jerry L. Frazier, Member  
Benjamin S. Lyons, Member  
Dorothy E. Maxwell, Member

**EATON COUNTY ROAD COMMISSION  
CHARLOTTE, MICHIGAN  
2017 HMA SURFACING PROPOSAL**

**CONTRACT D**

**To: Board of Eaton County Road Commissioners  
1112 Reynolds Road  
Charlotte, Michigan 48813**

**The undersigned has examined the Plans, Specifications, and the Location of the work described herein and is fully informed as to the nature of the work and the conditions relating to its performance, and understands that the quantities shown are approximate only and are subject to either Increase or Decrease.**

**The undersigned hereby proposes to furnish all necessary equipment, tools, apparatus, and other means of construction; do all the work and furnish all the materials; and, for the unit prices named in the itemized bid, complete the work herein described, in strict accordance with the plans thereof, and in strict conformity with the requirements of the 2012 Michigan Department of Transportation Standard Specifications for Construction and such other special provisions and supplemental specifications included as part of this proposal.**

**The undersigned further proposes to do such extra work as may be authorized by the Eaton County Road Commission, prices for which are not included in the itemized bid. Compensation shall be made on the basis agreed upon prior to commencing with extra work.**

**The undersigned encloses a certified or cashiers check on an open solvent bank, in the amount of \$25,000, payable to the Eaton County Road Commission as a guarantee of good faith. If the undersigned is the successful bidder and fails to enter into a contract to furnish satisfactory bonds to the Eaton County Road Commission within 15 days after being furnished with the necessary contract and bond forms, the Bid Deposit will be forfeited to the Eaton County Road Commission as liquidated damages. It is understood that the Bid Deposit of the successful bidder will not be returned until the contract has been executed and that the proposal guarantees of all other bidders will be returned promptly.**

**Company Name** \_\_\_\_\_

**Address** \_\_\_\_\_  
\_\_\_\_\_

**Phone #** \_\_\_\_\_

**Fax #** \_\_\_\_\_

**Signed by** \_\_\_\_\_

**Date** \_\_\_\_\_

EATON COUNTY ROAD COMMISSION

2017 HMA SURFACING PROGRAM  
 BID BLANK SUMMARY

05/17/2017

ROAD	FROM TO	LENGTH	WIDTH	UNIT													
				HMA APPLICATION RATE #/SYD	HMA, 36A TOP TON	HMA, 36A WEDGING TON	HMA, 36A LEVELING (140#) TON	HMA, 36A TOTAL STA	COLD MILLING HMA SURFACE SYD	PAVT MRKG, TYPE NR, 4 INCH, TEMP FT	MONUMENT BOX ADJUST EACH	PAVT, REM SYD	DRAINAGE STRUCT. ADJUST EACH	SUBBASE - LM CYD	AGGREGATE BASE, 6 INCH SYD	SLOPE RESTORATION SYD	SHOULDER, CL II TON
<b>CONTRACT D</b>																	
Springport	Bellevue to Spicerville	16275	23	190	3,950		2,910	6,860		977						1,790	
Brown	Allegan to Bismark	21356	24	190	5,410		3,990	9,400		1281	9					2,350	
Bismark	Brown to Moore	2700	24	190	680		500	1,180		162	2					300	
Gresham	M-100 to end of primary	5250	24	190	1,330		980	2,310	2,017	315	2					580	
Bridge	Canal to Dimondale VL	2155	22	190	500		370	870		129						240	
Old Lansing	Lansing Rd to Waverly	12688	22	100	1,600			1,600		761			31			1,860	
Gresham (millage)	Section Line to 1mi W. M-100	5290	22	220	1,490	330	140	1,960		317	3	250		100	700	300	780
Holmes (millage)	S. Royston Rd. to Wilson Rd.	10740	24	220	3,150	700	280	4,130		644	4					1,580	
<b>Total =</b>								<b>28,310</b>	<b>2,017</b>	<b>4586</b>	<b>20</b>	<b>250</b>	<b>31</b>	<b>100</b>	<b>700</b>	<b>300</b>	<b>9,480</b>

# EATON COUNTY ROAD COMMISSION

## 2017 HMA SURFACING BID BLANK

### CONTRACT D

ITEM OF WORK	QUANTITY	UNIT	UNIT PRICE	TOTAL AMOUNT
Pavt, Rem	250	SYD	\$ _____	\$ _____
Cold Milling HMA Surface	2,017	SYD	\$ _____	\$ _____
HMA, 36A	28,310	TON	\$ _____	\$ _____
Aggregate Base, 6 inch	700	SYD	\$ _____	\$ _____
Subbase - LM	100	CYD	\$ _____	\$ _____
Dr. Structure Cover, Adj.	31	EACH	\$ _____	\$ _____
Monument Box Adjust	20	EACH	\$ _____	\$ _____
Pavt Markg, Type NR 4 inch, Temp	4,586	FT	\$ _____	\$ _____
Shoulder, CI II	9,480	TON	\$ _____	\$ _____
Slope Restoration	500	SYD	\$ _____	\$ _____
<b>TOTAL BID CONTRACT D</b>				<b>\$ _____</b>

**The undersigned agrees that the quantities provided on the Contract D Bid Blank are estimates. Actual quantities paid will be based on final field measurements. The Eaton County Road Commission reserves the right to increase or decrease quantities, or add or eliminate entire projects from the contract. No unit price changes will be allowed as a result of increased or decreased quantities, or for added or eliminated projects.**

Company Name \_\_\_\_\_ Date \_\_\_\_\_

Address \_\_\_\_\_ Phone # \_\_\_\_\_

\_\_\_\_\_ Fax # \_\_\_\_\_

Signed by \_\_\_\_\_

EATON COUNTY ROAD COMMISSION

SPECIFICATIONS  
FOR  
**HMA SURFACING PROGRAM**

ECRC

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05/22/17

- A. All work included in the 2017 HMA Surfacing Program shall follow the 2012 Michigan Department of Transportation Standard Specifications for Construction, except as modified herein.
- B. Prepare the existing asphalt surface in accordance with section 501.03.C of the Standard Specifications. Joint cleanout will not be required. The contractor shall provide the Road Commission one week's notice prior to beginning pavement cleaning to provide for blading the shoulder or berm where necessary. Material left on the existing pavement resulting from the blading operations shall be removed as part of pavement cleaning. Payment for pavement cleaning shall be included in the unit price bid for HMA work items and will not be paid for separately.
- C. Place the bond coat in accordance with section 501.03.D of the Standard Specifications. Payment for the bond coat shall be included in the unit price bid for HMA work items and will not be paid for separately.
- D. Cold Milling HMA Surface shall be in accordance with section 501.03.C.5 of the Standard Specifications. Cold Milling HMA Surface includes edge milling or milling the entire road surface. The quantities and project locations where cold milling is indicated on the Bid Blank Summary are estimates only and will be paid based on final measurements. Any deletion or addition of project locations, or increases or decreases in milling quantities, will not change the unit prices bid. Cold milling will be paid by the square yard as COLD MILLING HMA SURFACE - SYD.

Where indicated on the bid blank summary, specified locations will be cold milled and the material salvaged and hauled to the Eaton County Road Commission garage closest to the milling operation. Refer to the COLD MILLING HMA SURFACE – SPECIAL special provision in the proposal.

Butt joints shall be milled wherever new HMA will abut adjacent pavement to remain. Payment of butt joints shall be included in the unit price bid for HMA work items and will not be paid for separately.

Edge milling will be used to remove the existing HMA surface at the curb line to a depth adequate to place the new surfacing flush with the curb edge at the yield shown on the bid blank. Where edge milling is indicated, the existing HMA surface shall be milled to a maximum width of 7 feet from the curb edge.

Where the entire road width is milled, the pavement shall be milled using automatic controls at 2% cross slope. The depth at the existing curb edge shall be as necessary to match the proposed yield. Where the milled depth creates a drop of 3" or greater, the contractor shall immediately place temporary ramps at all drives and butt joints and remove them immediately prior to surfacing. Payment for temporary ramps shall be included in the unit price bid for COLD MILLING HMA SURFACE - SYD or COLD MILLING HMA SURFACE, SPECIAL – SYD and will not be paid for separately. The contractor should expect to be milling into the subsurface on entire roads and on portions of some roads to obtain the specified cross section. Payment for milling subsurface material will be included in the payment for COLD MILLING HMA SURFACE – SYD.

- E. Adjust drainage structure covers, water valve boxes, and monument boxes in accordance with section 403.03 of the Standard Specifications. Quantities shown on the bid blank are estimates and payment will be for the actual number of covers adjusted.

Adjusting manhole covers, catch basin covers, and water valve manhole covers will be paid for as DR. STRUCTURE COVER, ADJ. - EACH. Monument box adjustments will be paid for as MONUMENT BOX ADJUST - EACH. Water valve boxes will be paid for as WATER SHUTOFF, ADJ. - EACH. Temporary lowering and/or protection of existing drainage structure covers for cold milling operations will be required, and shall be included in the unit price bid for COLD MILLING HMA SURFACE - SYD. Covers and boxes that do not require adjustment, but which require removal of the existing HMA surface (hand chipping) prior to surfacing will be paid for at 50% of the bid price for the respective adjust work item.

Drainage structure covers shall be adjusted with concrete blocks, bricks or concrete adjusting rings supported on a full bed of mortar. The exterior of all adjusted drainage structure covers shall be wrapped with non-woven geotextile fabric. Monument boxes shall be raised to the required elevation. Water valve boxes shall be adjusted by turning casting up to the required elevation.

Metal riser rings will not be allowed to adjust drainage structure covers, monument boxes, or valve boxes.

- F. Drainage structure reconstruction will be necessary on those projects designated on the bid blank or as determined by the Engineer and shall meet the requirements of section 403.03 of the Standard Specifications. Where the Engineer determines the existing masonry is deteriorated and must be reconstructed, the drainage structure shall be reconstructed. The drainage structure shall be reconstructed using new concrete blocks, bricks, or concrete adjusting rings supported on a full bed of mortar. The outside of the drainage structure shall be wrapped with non-woven geotextile fabric. All work, materials, and equipment necessary to perform drainage structure reconstruction will be paid as DRAINAGE STRUCTURE RECONSTRUCT - FT. Curb that must be removed and replaced to facilitate the pay item Drainage Structure Reconstruct will be paid for as CURB RECONSTRUCT - LFT.
- G. New curb and gutter will be required on those projects designated on the bid blank or as determined by the Engineer and shall be in accordance with section 802 of the Standard Specifications.

Where directed by the Engineer, existing curb and gutter shall be removed and replaced with curb and gutter of the same cross section. Work required will be removing the existing curb and gutter; excavating to prepare the grade; placing, grading, and compacting aggregate base for the new curb and gutter; constructing the curb and gutter; hand patching; and restoration. All work, labor, materials, and equipment for removing the existing curb and gutter, excavating, placing new aggregate base, placing new curb and gutter, hand patching, backfill and slope restoration will be paid for as CURB RECONSTRUCT – FT.

Where there is no existing curb and gutter, the grade will be excavated; aggregate base placed, graded, and compacted for the new curb and gutter and new HMA between the new curb and existing pavement; new curb and gutter constructed; and restoration. Refer to the Proposed Intersection Curb Construction detail. All work, labor, materials, and equipment for constructing the new curb and gutter where there is no curb and gutter will be paid for as CURB, CONC, DET B2 – SPECIAL. All work, labor, materials, and equipment for preparing the aggregate base including excavation between the existing asphalt to 6” behind the proposed curb and gutter, placing and compacting 6” aggregate base over the entire excavated area, backfill, and slope restoration will be

paid for as INTERSECTION GRADING – SYD. HMA placed between the new curb and gutter and existing pavement will be paid for as HMA, 36A.

- H. SUBBASE - LM and AGGREGATE BASE, 6 INCH - SYD shall be constructed in accordance with Sections 301 and 302 of the MDOT Standard Specifications.
- I. HMA APPLICATION RATES will be as shown on the bid blank summary.

All HMA (HMA mixtures) shall meet the requirements of the 2012 MDOT Standard Specifications for Construction, the attached Special Provisions, and as specified herein.

The final asphalt binder grade shall meet or exceed PG 58-28.

The HMA mix shall be designed at 4.00% air voids and field regressed to 3.5% air voids during production.

Prior to the start of production of any mixture, the contractor shall provide an MDOT approved mix design for any mix to be used and shall provide manufacturer certifications for all asphalt cement used on the project. All materials used shall be as specified in the MDOT Material Source Guide.

Wedging of uneven areas of the existing pavement prior to placing the surface course will be required as designated on the project summary. Wedging will be paid for at the unit price bid for HMA and will not be paid for separately. Bidders shall note that projects with wedging will include variable thicknesses placed in one pass. The bidders should expect the screed of the paver to be dragging stones during wedging operations. Application rates for wedging will not be increased to eliminate this condition.

- J. Aggregate shoulder construction shall be in accordance with section 307 of the Standard Specifications. Aggregate shoulder material shall meet MDOT Specification 23A. Aggregate shoulder construction will be required in rural and urban areas where there is no curb and gutter, as designated on the project summary. Aggregate shoulders shall be placed no wider than 2 feet unless otherwise directed by the Engineer. Compaction shall be performed using a rubber tired roller. Any spillage or surplus material shall be removed and the area shall be left with a clean-neat appearance. Additional aggregate shall be placed and compacted in gravel driveways as needed to build proper approaches and will be paid for at the unit price for SHOULDER, CL II – TON and will not be paid for separately.

The Eaton County Road Commission will test gravel furnished by the Contractor, and will reject gravel that does not meet the specifications.

The HMA pay items on each individual project will not be paid for until the aggregate shoulders for that project have been completed. Shoulder gravel will be paid for as SHOULDER, CL II - TON.

- K. Slope restoration shall be done in accordance with section 816 of the Standard Specifications. Slope restoration will be necessary in rural and subdivision areas that do not have curb and gutter, where directed by the Engineer. Topsoil shall be visually inspected and approved by the Engineer. Seed mixture shall be TGM. Seed shall be sowed into topsoil. Straw mulch and mulch anchoring shall be placed on the finished, seeded grade. Mulch blanket is an acceptable alternative for mulch and mulch anchoring. All labor, equipment, topsoil, seed, fertilizer, mulch, mulch anchoring, and mulch blankets will be paid for at the unit price bid for Slope Restoration and will not be paid for separately. Slope restoration will be paid for as SLOPE RESTORATION – SYD.

L. **TRAFFIC CONTROL** - Supply, install, and maintain the proper signs and the necessary traffic regulators per the MMUTCD, MDOT Maintaining Traffic Typicals, and as directed by the Engineer. No road closures will be allowed during paving operations and traffic regulators will be required to perform the work. On projects that include shoulder gravel or slope restoration, this requirement includes the furnishing, installation, maintenance, moving, and removal of low shoulder and all other signs and/or barricades needed to maintain traffic safety until the shoulders are placed and the project completed. Payment for traffic control shall be included in the bid prices for the other contract pay items and will not be paid for separately.

M. **Proof of ADEQUATE INSURANCE COVERAGE** by the successful bidder will be required as part of this contract. The successful Bidder shall furnish proof of Workers Compensation Insurance and a separate Liability Insurance Policy in favor of the Road Commission naming the Road Commission, its Commissioners, and Employees as an additional named insured, in the amount of \$1,000,000.

The successful Bidder will be required to provide a **PERFORMANCE BOND** in the amount of 50% of the total contract amount and a **LIEN BOND** in the amount of 50% of the total contract amount prior to award.

N. **PAY ESTIMATES** will be prepared twice monthly by the Engineer for the work completed on the project. Payment of 95% of such estimates will be made upon approval by the Road Commission. Final Payment will be made on an Approved Final Estimate after acceptance of the completed work by the Road Commission.

O. **Schedule:** The completion date for all work in this contract is **SEPTEMBER 22 , 2017**.

The Board reserves the right to cancel any of the individual projects and/or cancel either of the Contracts in its entirety. The Board reserves the right to add other similar projects to the contract. Any such cancellations or additions shall not change the unit prices bid and all unit prices bid shall remain firm through December 1, 2017.



### **PROJECT LOG**

The following is additional work necessary prior to performing the wedging and resurfacing of these roads.

Gresham Highway (Millage) – Approx. 6,500 feet west of M-100, remove the existing HMA at both ends of the fill section where directed by the engineer, place Granular Subbase, Aggregate Base, 6 inch, and HMA, 36A to fill the area of road settlement per the cross section. The grade change will vary from 0' at the ends to 1.8' at the deepest location. The final HMA width will be 22' with 2' aggregate shoulders on each side. Side slopes shall be 1:3. The Road Commission will provide grade and slope stakes.

West of the fill area for approx. 500 feet, place HMA, 36A leveling course utilizing slope control at 2% and 140#/syd. Place 190#/syd for the top course.

Gresham Highway – Approx. 4,200 feet west of M-100, cold mill 1.5" deep for the full width of the pavement for a length of approx. 530 feet. An additional 1.5" of depth shall be edge milled along the curb and gutter to provide the correct depth for the HMA yield of 330#/syd.

Brown Road and Bismark Highway – Along the entire length, place HMA, 36A leveling course at a yield of 140#/SYD utilizing slope control at 2% cross slope. This may require no material being placed at the centerline and greater than 1.5" placed on the edge. After the leveling course, place 190#/SYD of HMA, 36A for the surface course. The yield of the surface yield may be adjusted after the final yield of the level course is calculated.

MICHIGAN  
DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION  
FOR  
**RECYCLED HOT MIX ASPHALT MIXTURE ON LOCAL AGENCY PROJECTS**

CFS:KPK

1 of 2

APPR:JWB:CJB:03-13-14  
FHWA:APPR:03-13-14

**Add the following subsection to subsection 501.02.A.2, on page 234 of the Standard Specifications for Construction.**

- c. **Reclaimed Asphalt Pavement (RAP) and Binder Grade Selection.** The method for determining the binder grade in HMA mixtures incorporating RAP is divided into three categories designated Tier 1, Tier 2 and Tier 3. Each tier has a range of percentages that represent the contribution of the RAP binder toward the total binder, by weight. The tiers identified below apply to HMA mixtures with the following exception: Superpave mixture types E3, E3 High Stress, E10, E10 High Stress, E30, E30 High Stress, E50, and E50 High Stress used as leveling or top course must be limited to a maximum of 27 percent RAP binder by weight of the total binder in the mixture.

Recycled materials may be used as a substitute for a portion of the new materials required to produce HMA mixtures in accordance with contract.

- **Tier 1 (0% to 17% RAP binder by weight of the total binder in the mixture).** No binder grade adjustment is made to compensate for the stiffness of the asphalt binder in RAP.
- **Tier 2 (18% to 27% RAP binder by weight of the total binder in the mixture).** For all mixtures no binder grade change will occur in Tier 2 for all shoulder and temporary road mixtures.

The required asphalt binder grade must be at least one grade lower for the low temperature than the design binder grade required for the specified project mixture type. Lowering the high temperature of the binder one grade is optional. For example, if the design binder grade for the mixture type is PG 58-22, the required grade for the binder in the HMA mixture containing RAP would be a PG 52-28 or a PG 58-28.

For Marshall Mixes, no binder grade change will be required when Average Daily Traffic (ADT) is above 7000 or Commercial Average Daily Traffic (CADT) is above 700. No binder grade change will occur for LVSP, E03 and E1 mixtures used as leveling or top course.

The asphalt binder grade can also be selected using a blending chart for high and low temperatures. Supply the blending chart and the RAP test data used in determining the binder selection according to *AASHTO M 323*.

- **Tier 3 ( $\geq$  28% RAP binder by weight of the total binder in the mixture).** The

binder grade for the asphalt binder is selected using a blending chart for high and low temperatures per *AASHTO M 323*. Supply the blending chart and the RAP test data used in determining the binder selection.

EATON COUNTY ROAD COMMISSION

SPECIAL PROVISION  
FOR  
**SAMPLING ASPHALT BINDER**

ECRC:MMH

1 of 1

05-08-15

Original samples of asphalt binder shall be taken by the Contractor and delivered to the Engineer prior to incorporation into the mixture. The Contractor shall allow the Engineer access to witness the sampling. The frequency of sampling shall be each day material is placed as part of this contract unless otherwise determined by the Engineer. The cost of obtaining and delivering the samples to the Engineer will be included in the hot mix asphalt (HMA) pay items.

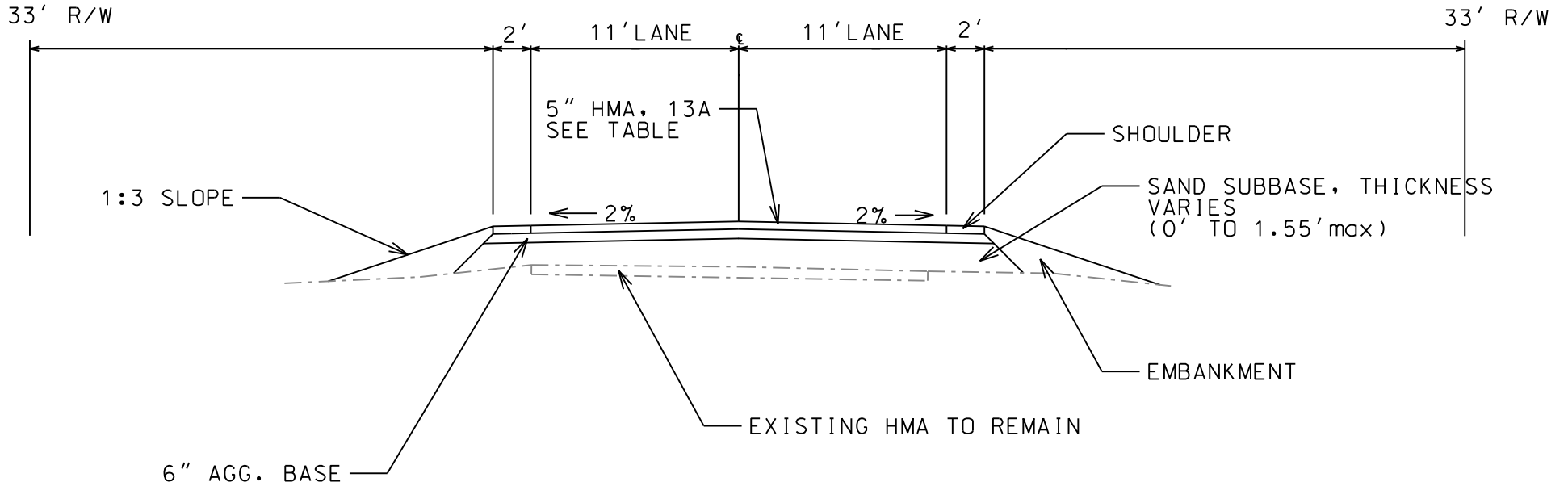
The Contractor must certify in writing that the materials used in the HMA mixture are from the same source as the materials used in developing the HMA mixture design and the bond coat is from an approved supplier as stated in MDOT's Material Quality Assurance Procedures Manual.

HMA APPLICATION TABLE

ITEM	RATE	ASPHALT PENETRATION	REMARKS
HMA 13A TOP	165 LB/SYD	PG 58-28	
HMA 13A LEVELING	165 LB/SYD	PG 58-28	
HMA 13A BASE	220 LB/SYD	PG 58-28	

HMA BOND COAT 0-0.01 GAL/SYD OR AS DIRECTED BY ENGINEER

TYPICAL SECTION  
NOT TO SCALE



ALL DISTURBED AREAS AND NEW SLOPES TO BE RESTORED WITH TOPSOIL 3", SEEDING, MIXTURE TDS, CHEMICAL FERTILIZER NUTRIENT, MULCH AND MULCH ANCHORING AS DIRECTED BY ENGINEER.

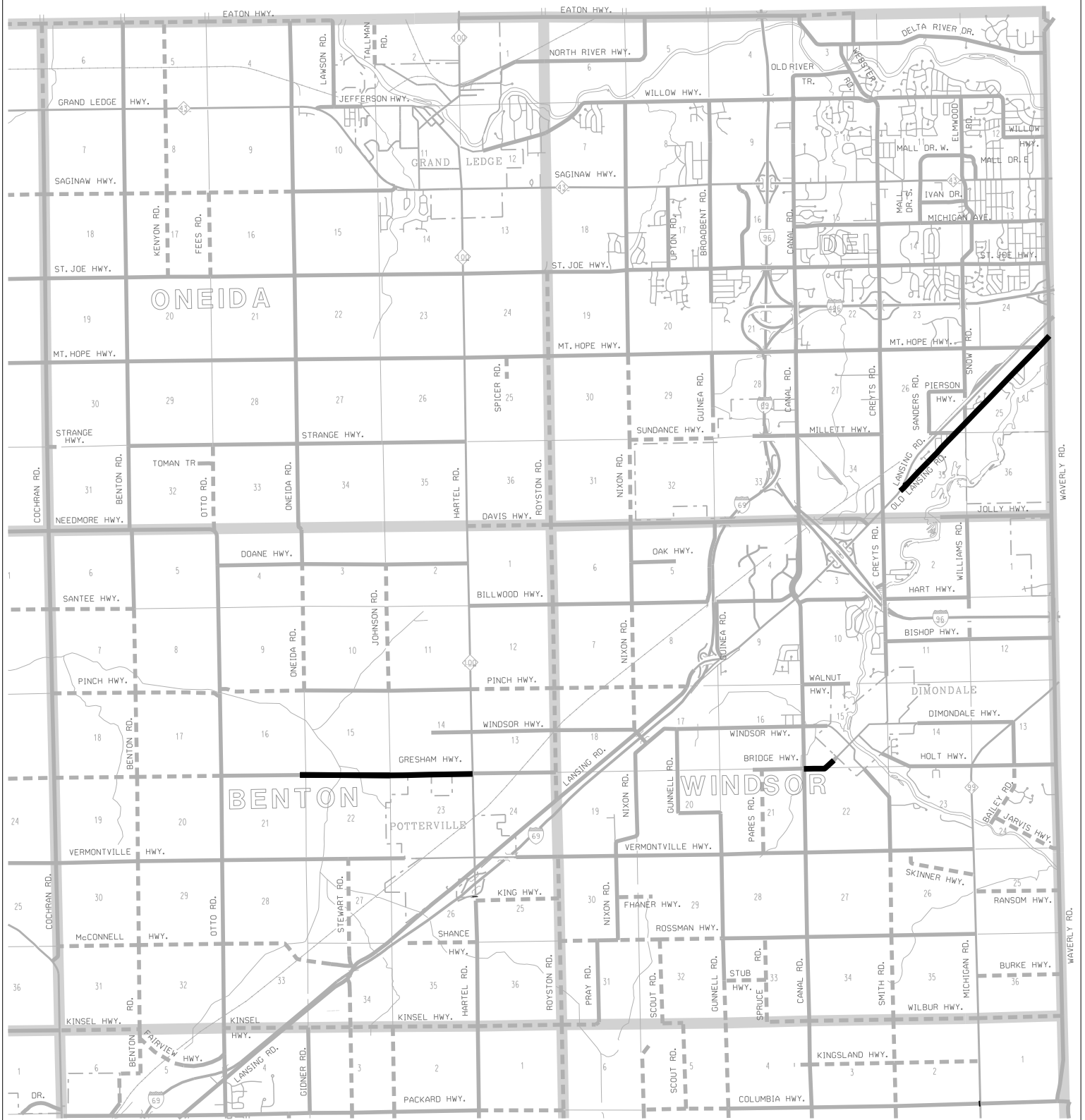
GRESHAM HWY. FILL SECTION			
DATE	NO SCALE	JOB NO.	DESIGN UNIT
05/04/2017		170045	M. HANNAHS

# EATON COUNTY, MICHIGAN 2017 HMA OVERLAY CONTRACT D

 ASPHALT OVERLAY

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May 17, 2017

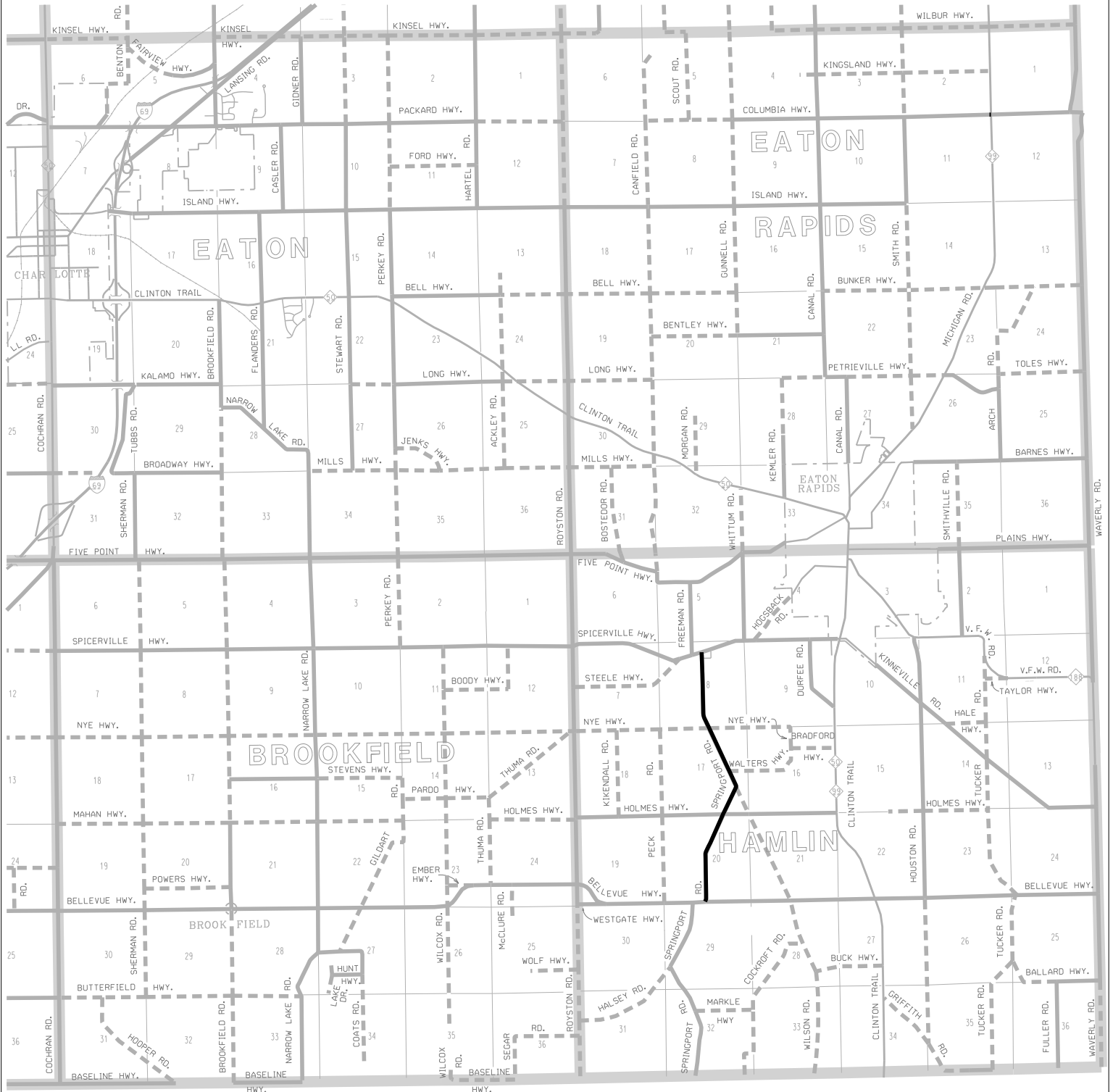


# EATON COUNTY, MICHIGAN 2017 HMA OVERLAY CONTRACT D

— ASPHALT OVERLAY

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May 17, 2017



# EATON COUNTY, MICHIGAN 2017 HMA OVERLAY CONTRACT D

— ASPHALT OVERLAY

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May 17, 2017

